



# DEVELOPMENT APPLICATION ACCESSIBILITY REPORT

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<b>Project Name</b>	Premium Car Park Area
<b>Project Address</b>	Newcastle Airport, Williamtown Drive, Williamtown NSW 2318
<b>Reference</b>	10322
<b>Revision</b>	4
<b>Date</b>	19/05/2022
<b>Attention</b>	Cox

## REVISION HISTORY

Revision	Date	Version	Prepared By	Reviewed By
1	04/04/2022	Initial Issue - draft	Nikki Jackson	Tom Clark
2	04/04/2022	Updated	Nikki Jackson	Tom Clark
3	04/05/2022	DA Issue	Nikki Jackson	Tom Clark
4	19/05/2022	Updated with CP numbers	Nikki Jackson	Tom Clark

### Document Disclaimer

This document has been prepared solely for the use of our client in accordance with our agreement for providing access consulting services. Although all due care has been taken in the preparation of this document, no warranty is given, nor liability accepted (except that required by law) in relation to the information contained within this document. The advice given is based on a professional judgement and an assessment of the information that could be derived at the time of the writing the report. Opinions, judgments and recommendations detailed in this document, are based on our understanding and interpretation of current statutory and regulatory obligations and standards and should not be construed as legal opinions. It is important to note that following the recommendations within this report will not in itself provide exemption from action under the DDA. The process of accessibility under the DDA is much broader than just the built environment; it covers management issues, staff approach and training and ongoing maintenance issues. It is important to note that as with all aspects of the built environment, there is often more than one way of resolving any issue identified. It is for the client to ultimately assess the recommendations put forward and fully assess their suitability for the proposal and the likely use(s) that they will be used and how the completed project will operate in practice.

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## 1.0 INTRODUCTION

Purely Access Pty Ltd has been engaged to provide access consultancy services for the proposed premium car park facility at Newcastle Airport, Williamtown Drive, Williamtown NSW 2318.

### 1.1 Purpose of the Report

This report forms part of the Development Application submission documents. The aim of this report is to provide feedback and confirmation that the proposals meet the principles of good accessible design and in turn demonstrate that it meets the key legislative and policy guidelines including

- Disability Discrimination Act 1992 (DDA)
- Disability Standards for Accessible Public Transport 2002 (DSAPT).
- Disability (Access to Premises-Buildings) Standards 2010 (Premises Standards)
- National Code for Construction Building Code of Australia Volume One 2019 Amendment 1 (BCA)
- AS1428.1:2001 edition - *Design for Access & Mobility -General requirements for access*
- AS1428.1:2009 edition - *Design for Access & Mobility -General requirements for access*
- AS1428.2: 1992 *Design for Access & Mobility –Enhanced and additional requirements – Building & Facilities*
- AS1428.4 1992 *Design for Access & Mobility. Tactile ground Surface indicators for the orientation of people with vision impairment*
- AS1428.4.1 2009 *Design for Access & Mobility. Means to assist the orientation of people with vision impairment- Tactile Ground Surface Indicators*
- AS1428.4.2 *Design for Access & Mobility. Means to assist the orientation of people with vision impairment- Wayfinding Signs*
- AS 2899.1 *Public Information Symbol Signs.*
- AS/NZS 1158 *Lighting for roads and public spaces*

This document does not constitute a report for the purposes of obtaining a Construction Certificate. The compliance comments in this report are for the purpose of the Development Approval lodgement. The level of detail provided for Development Approval is consistent with the design intent for this stage. Further assessment will be required at the Construction Certificate stage.

### 1.2 Project Details

The proposed development includes upgrades to the existing car park area, including the addition of accessible car parking spaces and a roof structure.

A car park is considered Class 7a under the National Construction Code Building Code of Australia and this facility is associated with a Class 9b transport building.

## 2.0 LEGISLATIVE REQUIREMENTS

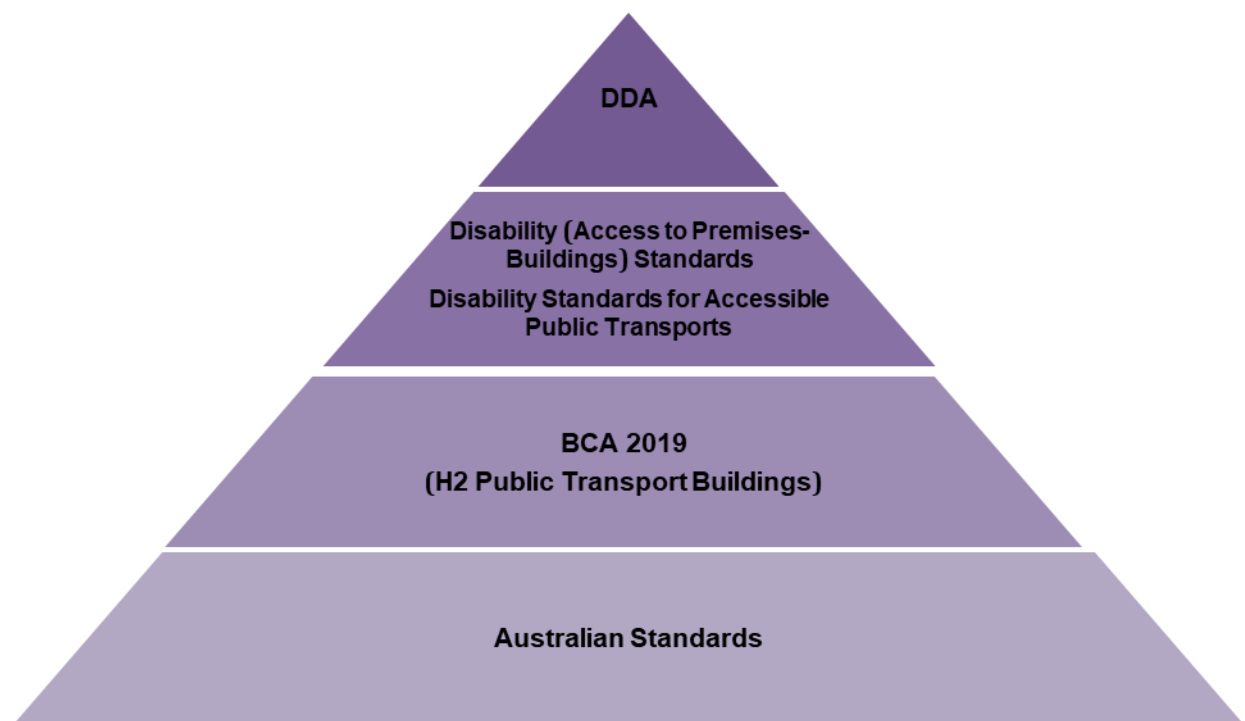
### 2.1 Disability Discrimination Act 1992

The Disability Discrimination Act 1992 (DDA) is federal legislation that makes it unlawful to discriminate against people with a disability, or associate in relation to access to and use of premises that the public enter or use to access goods, services and facilities. Public transport is a service covered by the DDA.

The DDA is a complaints based legislation, which is administered by the Australian Human Rights Commission (AHRC).

The DDA has enacted statutory instruments known as Disability Standards to provide a degree of clarity with respect to access to premises and facilities.

The figure below shows the hierarchy of legislation that is required be considered when assessing the existing structures and proposed scheme.



### 2.2 Disability Standards for Accessible Public Transport 2002

The purpose of the Disability Standards for Accessible Public Transport (DSAPT) is to enable public transport operators and providers to remove discrimination from public transport services. These standards apply to conveyances, premises and infrastructure and prescribe national requirements that public transport service providers and operators must meet in order to comply with the DDA in these areas.

DSAPT were amended when the Disability (Access to Premises – Buildings) Standards (Premises Standards) came into force to ensure that access requirements covered within the Premises Standards are no longer covered by the Transport Standards.

DSAPT references the following standards:

- AS 1428.1 2001 – Design for access and mobility Part 1: General requirements for access – New building work
- AS 1428.2 1992 - Design for access and mobility. Part 2: Enhanced and additional requirements – Buildings and facilities

- AS 1428.4 1992 - Design for access and mobility Part 4: Tactile ground surface indicators for the orientation of people with vision impairments
- AS 1735.12 1999 - Lifts, Escalators and moving walks. Part 12: Facilities for persons with disabilities.
- AS 2899.1 1986 - Public Information symbol signs. Part 1: General information signs.
- AS/NZ 3856.1 1998 - Hoists and ramps for people with disabilities – Vehicle mounted. Part 1: Product requirements.

However, since the DSAPT was originally written the following Standards have been updated:

- AS 1428.1 2009 - Design for access and mobility. Part 1: General requirement for access
- AS 1428.4.1:2009 - Design for access and mobility Part 4.1: Means to assist the orientation of people with vision impairment – Tactile Ground Surface Indicators

Application of the most up to date standards reference in the BCA is applied as part of the assessment process.

### 2.3 Disability (Access to Premises – Buildings) Standards 2010 & Building Code of Australia

The purpose of these Standards is to provide for equitable and dignified access to new buildings and those areas of existing buildings that undergo renovation or upgrade that require a building approval.

If a building complies with the Disability (Access to Premises-Buildings) Standards (Premises Standards) those responsible for the building cannot be subject to a successful complaint of unlawful discrimination under the Disability Discrimination Act (DDA) in relation to matters covered by the Premises Standards.

Building Certifiers, Building Developers and Building Managers all have obligations under the Standards and must ensure a building complies with the Standards, with each party being responsible for the area they have control over.

The Premises Standards align with the National Construction Code Building Code of Australia with the requirements being set out in the Access Code of the Premises Standards. However, the Premises Standards places additional requirements on existing buildings in that it recognises that in most circumstances it will be necessary to provide an accessible path of travel to the new or modified part and has therefore introduced the concept of 'affected part'.

An 'affected part' is the path of travel between and including the principal pedestrian entrance of an existing building to the new or modified part of the building. This path of travel must be a continuous accessible path of travel. The Premises Standards do not apply to an existing building until an application for approval of building work is submitted. It also recognises that it is not always possible to upgrade all existing buildings and therefore allows some exceptions and concessions.

### 2.4. Application of DSAPT & Premises Standards

When reading DSAPT the application of the standards applies differently to each element. The standards include reference to conveyances, premises and infrastructure. Therefore some elements will not apply to all three areas. For example, Part 4.1 of DSAPT requires a minimum width of 1800mm to passing areas, however, this only applies to infrastructure except airports that do not accept regular public transport services. It does not apply to the conveyance, or a premise as this would be covered by the Premises Standards.

It is important therefore to clearly define conveyance, premise and infrastructure. The definitions from DSAPT are as follows:

- **Conveyance** – A conveyance includes any of the following to the extent that they are used to provide a public transport service:
  - Aircraft,
  - Buses or coaches
  - Ferries
  - Taxis
  - Trains, trams, light rail, monorails, rack railways

- Any other rolling stock, vehicle or vessel classified as public transport within its jurisdiction by regulation or administrative action of any Government in Australia.

It does not include the following:

- Charter boats (including water taxis)
- Limousines (including chaffered hire cars)
- Self-drive rental cars
- **Infrastructure** – Infrastructure is any structure or facility that is used by passengers in conjunction with travelling on a public transport service. Infrastructure does not include any area beyond immediate boarding points (for example bus stop, wharves, ranks, rail stations, terminals).
- **Premises** – Premises are structures, buildings or attached facilities that an operator provides for passenger use as part of a public transport service. Premises are a form of infrastructure.

This report does not provide an assessment of any conveyance.

### 3.0 TECHNICAL ASSESSMENT

The following review is an assessment against the relevant sections of the Disability Standards for Accessible Public Transport, Disability (Access to Premises- Buildings) Standards, National Construction Code Building Code of Australia Volume One 2019 Amendment 1 (BCA), and referenced Australian Standards.

Car Parking is not part of DSAPT but is covered under the Premises Standards and the BCA.

#### 3.1 General Building Access Requirements

In accordance with Clause D3.1 of the BCA buildings and parts of buildings must be accessible in accordance with Table 3.1.

A continuous accessible path of travel is to be provided as follows:

Part Of Building	Accessibility Requirements
<b>Class 7a</b> Car parking	<ul style="list-style-type: none"> <li>To and within any level containing accessible car parking spaces</li> </ul>
<b>Class 9b</b> Other Assembly building	<ul style="list-style-type: none"> <li>To wheelchair seating spaces</li> <li>To and within all other areas normally used by the occupants, except that access need not be provided to tiers or platforms of seating areas that do not contain wheelchair seating spaces</li> </ul>



### 3.2 Accessible Car Parking

Requirement	Comments	Action Required
<b>Accessible Car Parking BCA Clause D3.5</b>		
<p>In accordance with Table D3.5 of the BCA, accessible carparking is required to be provided as follows.</p> <p><b>Class 7a car park</b> 1 space for every 100 car parking spaces or part thereof</p> <p><b>Class 9b</b> Other assembly building - Up to 1000 car parking spaces; 1 space for every 50 car parking spaces or part thereof</p>	<p>The proposals provide parking for 161 car parking spaces which includes 8 accessible car parking spaces.</p>	<p><b>Compliance Indicated</b></p>
<b>Design of Accessible Car Parking AS2890.6</b>		
<p><i>Accessible spaces are to be designed in accordance with AS2890.6-2009.</i></p> <ul style="list-style-type: none"> <li><i>Dimensions of angled accessible parking bays 2400 x 5400mm with adjacent 2400mm x 5400mm shared area and bollard in shared area.</i></li> <li><i>Dimensions of parallel parking bays 3200mm x 7800mm</i></li> <li><i>Height of vehicular path of travel to accessible parking space to be 2200mm and height above accessible parking space to be 2500mm</i></li> </ul>	<p>The accessible spaces are all shown with shared zones adjacent although the detail is not designed at this stage.</p> <p>The lowest point of the roof is indicated at a clear height of 2800mm.</p> <p>The accessible spaces are 2700mm wide by 5600mm with adjacent shared zones.</p>	<p><b>Compliance Indicated</b></p> <p>Details of line marking to be provided at Construction Certificate stage.</p>

### 3.3 Access Paths

Requirements
<p><b>Unhindered Passage</b> DSAPT Requirement - Part 2.1</p> <p>1) An access path that allows for unhindered passage must be provided along a walkway, ramp or landing.</p> <p>2) An access pathway must comply with <b>AS1428.2 (1992) Clause 8.1</b> except in premises to which the Premises Standards apply.</p> <p>AS1428.2 Clause 8.1 states:</p> <p>Walkways, ramps and landings shall comply with AS1428.1, with the following exceptions and additional requirements:</p> <ul style="list-style-type: none"> <li>(a) Walkways, ramps and landings shall have an unobstructed width of not less than 1200mm.</li> <li>(b) Ramps shall be provided with landings at the top and bottom of the ramp and at intervals not exceeding- <ul style="list-style-type: none"> <li>(i) for a ramp gradient of 1 in 14: 6m</li> <li>(ii) for a ramp gradient of 1 in 19: 14m</li> <li>(iii) for a ramp gradient between 1 in 19 and 1 in 14, at intervals which shall be obtained by linear interpolation.</li> </ul> </li> <li>(c) The dimensions of landings at doorways shall comply with the circulation spaces of AS1428.1 except that 100mm shall be added to all length (L) values and 50mm shall be added to all width (W) values.</li> </ul>
<p><b>Continuous Accessibility</b> DSAPT Requirement - Part 2.2</p> <p>An access path must comply with AS1428.2 Clause 7.</p> <p>AS1428.2 Clause 7 states:</p> <ul style="list-style-type: none"> <li>(a) Accessible paths of travel within the boundary of the site shall be provided from transportation stops, accessible parking and accessible passenger loading zones, and public streets or walkways to the accessible building entrance they serve.</li> <li>(b) Accessible paths of travel shall connect accessible buildings, facilities and spaces that are on the same site.</li> <li>(c) Accessible paths of travel shall connect accessible building entrances with all accessible spaces and facilities within a building.</li> <li>(d) Accessible paths of travel shall connect accessible entrances of each accessible building with those exterior and interior spaces and facilities that serve it.</li> </ul> <p>The accessible elements of buildings and facilities shall be arranged so as to minimise distances to be travelled between them.</p>
<p><b>Path branching into 2 or more parallel tracks</b> DSAPT Requirement - Part 2.3</p> <p>If an access path branches into 2 or more parallel tracks:</p> <ul style="list-style-type: none"> <li>a) the ends of each track must be on the main pedestrian traffic routes; and the parallel tracks must have equal convenience and be located as close as practicable to the main pedestrian branch.</li> </ul>
<p><b>Minimum unobstructed width</b> DSAPT Requirement - Part 2.4</p>

## Requirements

- 1) The minimum unobstructed width of an access path (**AS1428.2 (1992) Clause 6.4**, *Width of path of travel*).

Clause 6.4 states:

*The minimum clear width of a path of travel shall be 1200mm except at doors.*

- 2) However, the minimum unobstructed width of a moving footway may be 850 mm.

## Passing areas

DSAPT Requirements - Part 4.1

A passing area must have a minimum width of 1800 mm (**AS1428.2 (1992) Clause 6.5 (a)**, *Passing space for wheelchairs*).

Clause 6.5(a) states:

*The minimum width of a space required for two wheelchairs to pass each other shall be 1800mm.*

## Circulation space for wheelchairs to turn in

DSAPT Requirements - Part 3.1

A manoeuvring area must comply with **AS1428.2 (1992) Clause 6.2**, *Circulation space for a 180 degree wheelchair turn*.

Clause 6.2 states:

*The space required for a wheelchair to make a 180° turn shall be not less than 2070mm in the direction of travel and not less than 1540mm wide.*

## Access to Buildings

BCA Requirement - D3.2

An accessway must be provided-

- From the main points of a pedestrian entry at the allotment boundary
- From another accessible building connected by a pedestrian link
- From any required accessible car parking space

## Findings

The proposed pathways provide a continuous accessible path of travel from the car park area (including from the accessible car parking spaces) to the principal entrances of the terminal. The pathways exceed the minimum widths required and allow for turning and passing. Pathways also connect into existing infrastructure.

## Action

### Compliance indicated

Detailed landscape drawings will be required at Construction Certificate stage, Kerb ramps are to be provide to any raised pathway areas.

### 3.4. Resting Points

#### Requirements

##### Resting Points DSAPT Requirements - Part 5.1

(1) There must be resting points for passengers along an access path of the walking distance between facilities or services exceeds 60m (**AS1428.2 Note to Clause 7, Continuous accessible path of travel**).

Note to Clause 7 states:

NOTE: Attention to the siting of facilities and clear information signs directing people to these facilities will greatly reduce the fatigue experienced by people with disabilities. Limitations on stamina, which can result in fatigue, shortness of breath and dizziness, are posed by many disabilities such as cardio-pulmonary disorders, haemiplegia and amputation. Where there are unavoidable distances between facilities, frequent resting points with seats, handrails, and drinking-water fountains or similar outlets should be provided.

In areas of high use by people with ambulatory disabilities, such as areas frequented by elderly people, seats should be provided no more than 60 m apart alongside paths of travel.

The following table shows the demonstrated ability of people with disabilities to move more than a stated distance without a rest.

Type of disability	Percentage unable to move more than the stated distance m				
	18	68	137	180	360
People who use wheelchairs	0	5	5	60	85
People with vision impairment	0	0	5	50	75
People who use walking aids	10	25	40	80	95
Ambulatory people	5	15	25	70	80

(2) A resting point must provide seats (AS1428.2 Clause 27.1a Street furniture).

AS1428.2 Clause 27.1 a states:

General street furniture, which includes objects such as seats, tables, drinking fountains, planter boxes, rubbish bins and the like, shall comply with the following:

- (a) *Objects shall not protrude into an accessible path of travel. Seats shall be a minimum of 500mm away from the path of travel.*
- (b) *Objects shall be of a colour which provides a contrast with their background and have a luminance factor of not less than 0.3 (30%).*

#### Findings

The car park area is not subject to the requirements of DSAPT, however consideration should be given to implementing rest seating as part of the overall site wide strategy. No rest seating is provided within the car park area, however rest seating will be provided to the terminal forecourt.

#### Action

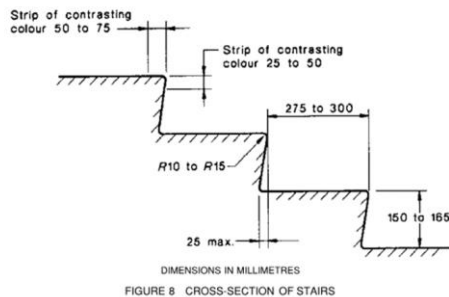
N/A

### 3.5. Ramps

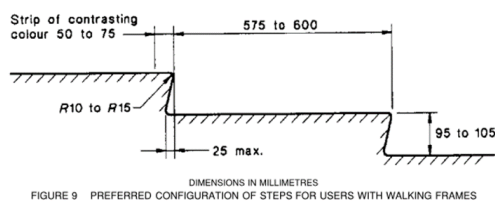
Requirements
<p><b>Ramps on access paths</b> <b>DSAPT Requirements - Part 6.1</b></p> <p>A ramp on an access path must comply with <b>AS1428.2 (1992) Clause 8</b>. Clause 8 states:</p> <p>8.1 Walkways, ramps and landings shall comply with AS1428.1, with the following exceptions and additional requirements:</p> <ol style="list-style-type: none"> <li>Walkways, ramps and landings shall have an unobstructed width of not less than 1200mm.</li> <li>Ramps shall be provided with landings at the top and bottom of the ramp and at intervals not exceeding- <ul style="list-style-type: none"> <li>for a ramp gradient of 1 in 14: 6m</li> <li>for a ramp gradient of 1 in 19: 14m</li> </ul> for ramp gradients between 1 in 19 and 1 in 14, at intervals which shall be obtained by linear interpolation.</li> <li>The dimensions of landings at doorways shall comply with the circulation spaces of AS1428.1 except that 100mm shall be added to all length (L) values and 50mm shall be added to all width (W) values.</li> </ol> <p>8.2 Outdoor conditions. In outdoor conditions, walkways, ramps and landings shall be designed so that water does not accumulate on surfaces.</p> <p>8.3 Ramp handrails to comply with Clause 10 (AS1428.2)</p> <p>8.4 Kerb ramps and step ramps</p> <p>8.4.1 the design, construction and location of kerb ramps and step ramps shall comply with AS1428.1.</p> <p>8.4.2 Wherever a path of travel crosses a kerb, one of the following shall be provided-</p> <ol style="list-style-type: none"> <li>a kerb ramp in accordance with AS1428.1 (preferred gradient, see notes to figure 8 of AS1428.1.</li> <li>ramps and landings in accordance with this Clause (8); or</li> <li>the surfaces shall be graded to meet each other, provided that any gradient and abutment of surfaces complies with AS1428.1 and that tactile directional indicators are provided as appropriate.</li> </ol> <p>8.4.3 Kerb ramps (the entry to ramps) at marked crossings shall be wholly contained within the markings, excluding any flared sides.</p> <p>8.4.4 Raised islands in crossings shall be cut through level with the street or have kerb ramps at both sides and a level area at least 1220mm long in part of the island intersected by the crossing.</p> <p>8.4.5 The ramp and sloping sides shall be slip resistant and of a colour that contrasts with the adjoining surface.</p> <p>8.4.6 Warning strips shall be provided at the top of the ramp, in accordance with clause 18.1.</p> <p><b>BCA Requirements – D3.3 &amp; H2.3</b></p> <p>In a building required to be accessible-</p> <ol style="list-style-type: none"> <li>Every ramp and stairway, except for ramps and stairways in areas exempted by D3.4, must comply with- For a ramp, except a fire isolated ramp clause 10 of AS1428.1; <ul style="list-style-type: none"> <li>A ramp forming part of an accessway must comply with clause 8 of AS1428.2</li> <li>The requirements of D3.11(a) do not apply to Class 9b or Class 10 public transport buildings</li> </ul> </li> </ol>
Findings
<p>The gradients to the pathways are such that no ramps are required. Kerb ramps are required to any footpaths which are not at grade.</p>

Action
<b>Capable of Compliance</b> Kerb ramps are to be provided to footpaths which are not at grade.





- Steps shall have a warning strip at the nosing's as shown in Figures 8 & 9 (AS1428.2)



## BCA Requirements – H2.7

Stairs must comply with

- Clause 9.1 of AS1428.1 2001
- Clause 9.2 of AS1428.1 2001
- Clause 13.2, 13.3 and figures 8 & 9 of AS1428.2 1992.

## Findings

No stairs are indicated to the car park.

## Action

N/A



### 3.7. Surfaces

Requirements
<p><b>DSAPT Requirements - Part 10.1</b></p> <p>(1) Ground and floor surfaces must comply with <b>AS1428.2 (1992) Clause 9</b>, <i>Ground and floor surfaces</i>.</p> <p>(2) <b>AS1428.1 Supplement 1 (1993) Clause C12</b> provides criteria for the selection of floor surfaces.</p> <p>AS1428.2 Clause 9 states:</p> <p><i>Ground and floor surfaces shall comply with the requirements for floor surfaces in AS1428.1, and with the following:</i></p> <ul style="list-style-type: none"> <li>a) <i>Paving bricks with bevelled edges or chamfered arises and heavily textured and figured surfaces such as raked joints pavers shall not be used.</i></li> <li>b) <i>Where carpet is included on ground or floor surface, the following requirements apply:</i> <ul style="list-style-type: none"> <li>i. <i>The carpet shall be securely attached</i></li> <li>ii. <i>Any pad, backing or cushioning shall provide a firm surface</i></li> <li>iii. <i>The carpet shall have a level loop, a textured loop, a level cut pile or level cut or uncut pile texture.</i></li> <li>iv. <i>The pile height shall be no more than 6mm</i></li> <li>v. <i>Exposed edges of carpets shall be fastened to the floor surface and shall have a trim along the entire length of any exposed edge.</i></li> <li>vi. <i>Carpet edge trim shall create no ridge on the floor surface higher than 3mm.</i></li> </ul> </li> <li>c) <i>If gratings are located in a walking surface, they shall have spaces not more than 13mm wide and not more than 150mm long. If gratings have elongated openings, they shall be placed so that the long dimension is traverse to the dominant direction of travel.</i></li> </ul>
Findings
<p>Surfaces finishes will generally comprise of concrete and paving.</p>
Action
<p><b>Capable of Compliance.</b></p> <p>Detailed design information including slip resistance properties are to be provided at detailed design stage.</p>

### 3.8. Signs

Requirements
<b>International symbols for accessibility and deafness</b> <b>DSAPT Requirements - Part 16.1</b>
<ol style="list-style-type: none"> <li>1) The international symbols for accessibility and deafness (AS1428.1 (2001) Clause 14.2, International symbol and Clause 14.3. International symbol for deafness) must be used to identify an access path and which facilities and boarding points are accessible.</li> <li>2) The colours prescribed in AS1428.1 (2001) Clause 14.2 (c) are not mandatory.</li> <li>3) The size of accessibility symbols must comply with AS1428.2 (1992) Table 1.</li> </ol>
<b>Height and illumination</b> <b>DSAPT Requirements - Part 17.1</b>
<p>Signs must comply with AS1428.2 (1992) Clause 17.1, Signs, Clause 17.2, <i>Height of letters in signs and Clause 17.3, Illumination of signs and Figure 30.</i></p> <p>Generally signs should be in accordance with AS1428.1 and be appropriately illuminated.</p>
<b>Location – premises and infrastructure</b> <b>DSAPT Requirements - Part 17.2</b>
<p>Signs must be placed according to AS1428.2 (1992) Clause 17.4, <i>Location of signs.</i></p> <p>Clause 17.4 states:</p> <p><i>Signs including symbols, numbering and lettering should be located as follows:</i></p> <ol style="list-style-type: none"> <li>a) <i>Where they are clearly visible to people in both seated and standing position.</i></li> <li>b) <i>At changes in direction</i></li> <li>c) <i>At sites where directional decisions are made, to enable the appropriate decisions to be made before a change of direction occurs.</i></li> <li>d) <i>Where the surface of the wall surrounding the sign provides sufficient contrast to the sign. If this surface provides insufficient contrast (e.g. patterned wallpaper), the background to the sign shall be increased in size.</i></li> </ol>
<p>Figure 1 - Zones for viewing and for common viewing</p>
<b>Electronic notices</b> <b>DSAPT Requirements - Part 17.5</b>
<ol style="list-style-type: none"> <li>1) Presentations of words or numbers on electronic notices must be visible for at least 10 seconds, unless the electronic notice is for the purpose of ticket validation.</li> </ol>

## Requirements

- 2) If the electronic notice is for this purpose, the words or numbers on the notice must cease to be visible before the end of 10 seconds if the ticket validation device is used by another person within that time.

## Raised lettering or symbols or use of Braille

DSAPT Requirements - Part 17.6

- 1) If a sign incorporates raised lettering or symbols, they must be at least 0.8mm above the surface of the sign.
- 2) If an operator or provider supplements a notice with Braille characters, they must be placed to the left of the raised characters.

## BCA Requirement - D3.6

Signage in accordance with specification D3.6 is required as follows:

- To identify unisex and ambulant sanitary Facilities
- To identify each door required by BCA Clause E4.5 to be provided with an exit sign, stating 'EXIT' and 'Level' number
- Where a pedestrian entrance is not accessible, signage must be provided to indicate the location of the accessible entry
- Identify areas containing hearing augmentation
- Buildings subject to F2.9, directional signage to be provided at each bank of sanitary facilities and at accessible unisex sanitary facility to direct a person to the nearest accessible adult change facility.

## Findings

The signage has not been fully documented at this stage of the design. Wayfinding and information signage will be required.

## Action

### Capable of Compliance

Signage to be reviewed at Construction Certificate stage. A wayfinding strategy will need to be developed.

### 3.9. Tactile Ground Surface Indicators

Requirements
<b>Location</b> <b>DSAPT Requirements - Part 18.1</b>
<p>Tactile ground surface indicators (TGSIs) must be installed on an access path to indicate stairways, ramps, changes of direction, overhead obstructions below a height of 2000mm, and hazards within a circulation space or adjacent to a path of travel (AS1428.2 (1992) Clause 18.1, Tactile ground surface indicators).</p> <p>Clause 18.1 states:</p> <ol style="list-style-type: none"> <li><i>TGSIs shall be provided at the following locations:</i></li> <li><i>Stairways, escalators and ramps</i></li> <li><i>Kerb ramps and step ramps</i></li> <li><i>Pedestrian crossings at roadways</i></li> <li><i>Pedestrian crossings in high use vehicular areas e.g. car parks</i></li> <li><i>Vehicle pick up and drop off areas</i></li> <li><i>Railway platforms</i></li> <li><i>Passenger wharves</i></li> <li><i>Where there is a hazard within a circulation space or adjacent to a path of travel.</i></li> <li><i>Where indication of a change in direction is required.</i></li> </ol>
<b>Style and dimensions</b> <b>DSAPT Requirements - Part 18.2</b>
<ol style="list-style-type: none"> <li>The style and dimensions of tactile ground surface indicators must comply with AS1428.4 (1992).</li> <li>The stated dimensions may be reduced where a conveyance design does not provide the necessary area.</li> </ol> <p>Note: AS1428.4 has been revised twice and as it is recognised that the latest revision AS1428.4.1 2009 is consistent with the intent of DSAPT this is the standard that will be followed.</p>
<b>BCA Requirements – D3.8</b>
<p>Tactile indicators will be required in the following locations:</p> <p>Stairways (other than fire isolated stairways)</p> <ul style="list-style-type: none"> <li>Ramps (other than a fire isolated, step or kerb or swimming pool ramp), An escalator</li> <li>A passenger conveyor or moving walkway</li> <li>Where an overhead obstruction is less than 2m (except doorways) if no suitable barrier is provided</li> <li>Where an accessway adjacent to a pedestrian entrance meets a vehicular way if there is no kerb or kerb ramp at that point</li> </ul> <p>TGSIs are to be selected and installed in accordance with AS1428.4.1 2009.</p>

Findings
No tactile indicators are currently indicated in the design.

Action
<b>Capable of Compliance.</b>
<p>TGSIs are to be documented at detailed design stage. A strategy with respect to wayfinding as well as hazard TGSIs should be considered.</p>

### 3.10. Lighting

Requirements																			
<b>Illumination levels – premises and infrastructure</b> <b>DSAPT Requirements - Part 20.1</b>																			
<p>Any lighting provided must comply with minimum levels of maintenance illumination for various situations shown in the notes to AS1428.2 Clause 19.1.</p> <p>Clause 19.1 states:</p> <p><i>Illumination levels shall be uniform and comply with the requirements for maintenance illumination set out in AS1680.2.</i></p> <p><i>The following minimum levels of maintenance illumination are recommended:</i></p> <table> <tr> <td><b>Entrances</b></td><td></td></tr> <tr> <td>Passageways and walkway</td><td>s150 lx</td></tr> <tr> <td>Stairs</td><td>150 lx</td></tr> <tr> <td>Ramps</td><td>150 lx</td></tr> <tr> <td>Lifts</td><td>See <a href="#">AS 1735.12</a></td></tr> <tr> <td>Toilet and locker rooms</td><td>200 lx</td></tr> <tr> <td>Counter tops</td><td>250 lx</td></tr> <tr> <td>General displays</td><td>200-300 lx</td></tr> <tr> <td>Telephones</td><td>200 lx</td></tr> </table>		<b>Entrances</b>		Passageways and walkway	s150 lx	Stairs	150 lx	Ramps	150 lx	Lifts	See <a href="#">AS 1735.12</a>	Toilet and locker rooms	200 lx	Counter tops	250 lx	General displays	200-300 lx	Telephones	200 lx
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Findings																			
Lighting will be provided to the car park area and adjoining road and pedestrian routes.																			
Action																			
<b>Compliance Indicated.</b>																			

### 3.11. Street furniture

Requirements
<b>Seats</b> <b>DSAPT Requirements - Part 23.1</b>
<p>Seats must comply with AS1428.2 (1992) Clause 27.2, Seating in pedestrian areas.</p> <p>Clause 27.2 states:</p> <ul style="list-style-type: none"> <li>• Seats should generally be 450mm high but where a proportion of elderly users are anticipated, heights up to 520mm are preferred. Children and small people may prefer a seat as low as 350mm. Where possible a range of seat heights should be provided.</li> <li>• The front of the seat shall have a clear space between any legs at ground level to within 150mm of the front edge of the seat, and to within 100mm of the seat height to allow for rearward adjustment of feet when rising.</li> <li>• Where armrests are provided, the top surface of the armrests shall be of a height of 260±40mm above the seat.</li> <li>• The front edge of the seat shall have a minimum radius of 30mm</li> <li>• No edge or projection shall have a radius of less than 5mm unless protected from contact with the user.</li> <li>• The seat shall drain free of water.</li> </ul>
<b>Poles and obstacles etc</b> <b>DSAPT Requirements - Part 2.5</b>
<p>(1) Poles, columns, stanchions, bollards, and fixtures must not project into an access path.</p> <p>(2) Obstacles that abut an access path must have a luminance contrast with a background of not less than 30%.</p>
Findings
<p>Limited details are provided at this stage of the design, however minimal landscaping is provided as part of this scope of works.</p>
Action
<b>Capable of Compliance</b> <p>To be detailed at detailed design stage as applicable.</p>

## 4.0 DRAWINGS REVIEWED

The following drawings/ documents have been considered in the formulation of this report:

Drawing No.	Revision	Title
ACE-01-AR-DRG-DA-00-01	A	Premium Carpark – Cover Sheet
ACE-01-AR-DRG-DA-00-02	A	Premium Carpark – Existing Site Plan
ACE-01-AR-DRG-DA-00-03	A	Premium Carpark – Demolition Plan
ACE-01-AR-DRG-DA-00-04	A	Premium Carpark – Site Plan
ACE-01-AR-DRG-DA-00-05	A	Premium Carpark – Floor Plan
ACE-01-AR-DRG-DA-00-06	A	Premium Carpark – Floor Plans-Stage 1
ACE-01-AR-DRG-DA-00-07	A	Premium Carpark – Floor Plans-Stage 2
ACE-01-AR-DRG-DA-00-08	A	Premium Carpark – Floor Plans-Stage 2
ACE-01-AR-DRG-DA-00-09	A	Premium Carpark – Sections
ACE-01-AR-DRG-DA-00-10	A	Premium Carpark – Signage Plans

## 5.0 CONCLUSION

In the professional opinion of Purely Access Pty Ltd the proposals are capable of meeting the requirements the Disability Standards for Accessible Public Transport, Disability (Access to Premises-Buildings) Standards, and the Performance Requirements set out in the National Construction Code Building Code of Australia Volume One 2019 Amendment 1 (BCA) and referenced Australian Standards with respect to access for people with a disability. Further design information focusing on the detailed elements will be developed as the scheme progresses through to the construction phase to ensure compliance is achieved.

If you have any further queries in relation to the report please contact the undersigned.

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